

Agenda

ANC 6A Transportation & Public Space Committee Meeting

Monday, April 20, 2026 at 7:00 pm

Virtual Meeting via Zoom

For those attending via Zoom, use this link: <https://dc-gov.zoom.us/j/87094450940>

Call-in Number: 1 301 715 8592

Webinar ID (access code): 870 9445 0940

One tap mobile: +19292056099,,87094450940#

Public Meeting – All are welcome

Community comment welcome; may be limited to two minutes to provide opportunity for all to speak. Community comment time will be opened after each Old and New Business item.

- I. Call meeting to order.
- II. Introductions & Announcements.
- III. Old Business
 - a. Public Space Construction Permit application #484855 at 800 - 899 BLOCK OF 10TH STREET NE, WASHINGTON, DC ,1000 - 1099 BLOCK OF H STREET NE, WASHINGTON, DC has been resubmitted by the applicant according to permit office's direction.
- IV. New Business
 - a. Anwar Saleem of H Street Main Street will present a grant application. He is requesting ANC support for it.
 - b. Electric Alley Renaming Request - Community member Paul Angelone is requesting that the alley at D Street NE and Kingsman Field and Dog Park between 13th Street NE and Tennessee Avenue NE be officially named "Electric Alley." This is its historical name from the early 1900's when the field was a car barn.
 - c. Art Proposal for Lovejoy Park (12th and E Streets NE) - Duilio Pasariello, a member of DDOT's street advisory panel, joins the committee to present a proposal for art installations on the lampposts at Lovejoy Park similar to those at J Cole Recreation Center and Park.
 - d. DDOT NOI 26-69-MESD - Safety Treatment at Bladensburg Road NE between Benning Road and Neal Street NE - This is technically in ANC 5C but it borders very close to Starburst plaza.
 - e. ANC 5C's Resolution for Equitable Metrorail Access - This is a resolution in support of WMATA's previously published metrorail expansion options. Some of the metro plans put forward also involved metro stops in 6A.
- V. Additional community comment (time permitting).
- VI. Adjourn meeting.

PUBLIC SITE PLAN KEYNOTES:

- NEW TREE PLANTING LIQUIDAMBAR STRYACIFLUA "SLENDER SILHOUETTE" - MAX. 3' CAL
- 10' WIDE CONCRETE SIDEWALK REPAIR, PER DDOT STREETScape STANDARDS. PROVIDE STRUCTURAL SOIL.
- CLOSING EX. APRON & PROVIDE 5' WIDE BRICK SIDEWALK, PER DDOT STREETScape STANDARDS. PROVIDE STRUCTURAL SOIL.
- NEW GREEN SPACE WITH 42" GUARDRAIL.
- RELOCATED BIKE RACKS.
- NEW BIKE RACK.
- WALKWAY FROM SIDEWALK TO CELLAR STAIR
- STAIR TO CELLAR WITH GUARD RAIL.
- EXISTING PROJECTION.
- RETAIL ENTRY FROM SIDE WALK.
- NEW ORIEL PROJECTION.
- NEW GREEN SPACE.
- STORM RUNOFF FROM PERMEABLE PAVEMENT PARKING AREA TO BE CAPTURED AT PROPERTY LINE BY TRENCH DRAIN, PER DDOT IPMA REQUIREMENTS.
- NEW PROJECT STARTING @ 2ND FLOOR
- EXISTING PROJECTION START AT CELLAR LEVEL
- NEW TREE - AMERICAN SYCAMORE - MAX. 3" CAL.
- TREE & EX. STANDARD PAVER TO BE REMOVED AND RE INSTALLED AFTER STRUCTURAL SOIL VOLUME PROVIDED.

PUBLIC SPACE IMPROVEMENT DETAILS:

No.	Improvement	Dimensions	Level / Location	Remarks
1	Walkway to cellar	4'-0" x 10'-0"	1st Floor	
2	Concrete walk to cellar	15'-0" x 4'-0"	Ground to Cellar	
3	Relocate tree	6'-0" x 6'-0"	1st Floor	
4	New Projection	6'-4" x 2'-10"	Starting and Existing projection	
5	Projection	9'-0" x 3'-11"	Starting and Existing projection	
6	Projection	16'-11" x 3'-4"	1st Floor	Existing projection replaced with new metal roof
7	Projection	24'-2" x 3'-4"	1st Floor	Existing projection replaced with new metal roof
8	New Oriel Projection	17'-0" x 2'-4"	Existing and Proposed projection	
9	New Oriel Projection	17'-0" x 2'-4"	Existing and Proposed projection	
10	Cladding existing	29'-11" x 4'-0"	1st Floor	Convert to brick sidewalk w/ 3" CAL
11	New Tree	6'-0" x 6'-0"	1st Floor	100% Native Tree
12	New Tree	6'-0" x 6'-0"	1st Floor	100% Native Tree

No.	Improvement	Dimensions	Location	Remarks
1	New Green Space	36' 0" x 15' 0"	1000 S	North part
2	42" Guard Rail	44' 0" x 3' 0"	1000 S	South part
3	New Green Space	3' 0" x 6' 0"	1000 S	
4	New Green Space	3' 0" x 6' 0"	1000 S	
5	New Green Space	3' 0" x 6' 0"	1000 S	
6	New Tree	3' 0" x 3' 0"	1000 S	

PUBLIC SPACE SITE PLAN NARRATIVE

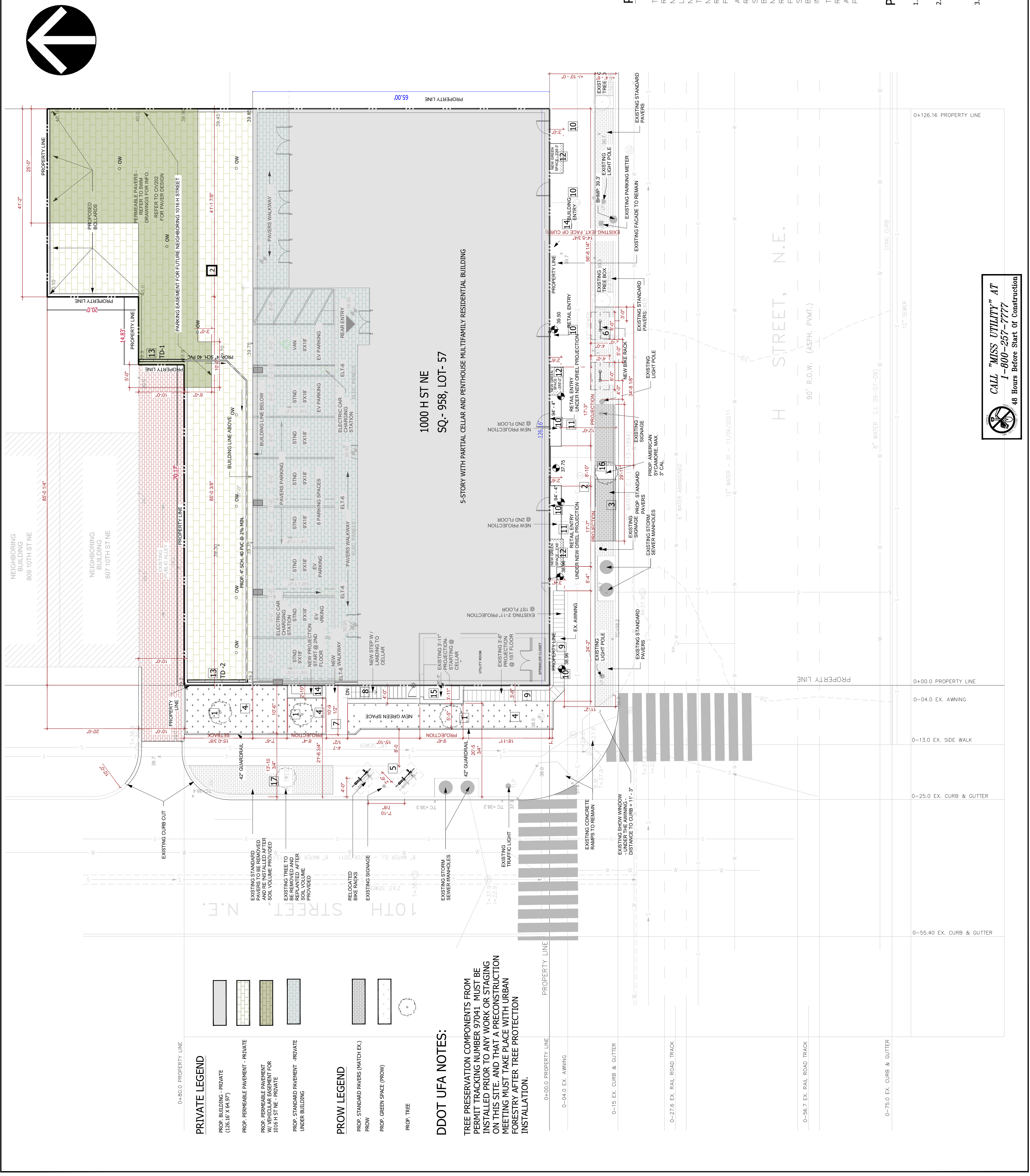
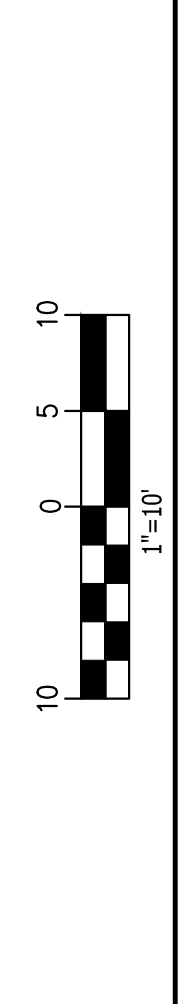
THE PUBLIC SPACE IMPROVEMENTS FOR THIS PROJECT INCLUDE A RANGE OF UPGRADES ALONG BOTH 10TH STREET NE AND H STREET NE. ALONG 10TH STREET, THE WORK WILL PROVIDE A NEW LANDSCAPED GREEN AREA ENCLOSED WITH A 42-INCH GUARDRAIL, A NEW STAIRWAY TO THE CELLAR LEVEL, AND A WALKWAY LEADING TO THE PROPOSED CELLAR ENTRANCE. THE DESIGN ALSO INTRODUCES A NEW ORIEL WINDOW PROJECTION BEGINNING AT THE SECOND FLOOR, RELOCATES THE EXISTING BIKE RACK, AND REPLACES THREE EXISTING PROJECTIONS WITH NEW METAL ROOFS.

ALONG H STREET NE, THE EXISTING APRON WILL BE CLOSED AND REPLACED WITH A NEW BRICK SIDEWALK TO MATCH THE EXISTING STREETScape. INCORPORATING A FOUR-FOOT-WIDE TREE PLANTING BOX, THE CONCRETE PORTION OF THE APRON WILL BE REPAIRED WITH NEW CONCRETE, WHILE THE EXISTING PROJECTION WILL HAVE ITS ROOF REPLACED WITH METAL. ADDITIONALLY, TWO NEW ORIEL WINDOW PROJECTIONS WILL BE CONSTRUCTED BEGINNING AT THE SECOND-FLOOR LEVEL. NEW STREETScape ELEMENTS, INCLUDING A BIKE RACK AND THREE LANDSCAPED GREEN SPACES, INTEGRATED INTO THE BUILDING FRONTAGE—WILL BE INSTALLED.

TO COMPLY WITH DDOT IPMA STORMWATER MANAGEMENT REQUIREMENTS, RUNOFF FROM THE REAR PERMEABLE PAVEMENT AREA WILL BE CAPTURED BY A TRENCH DRAIN SYSTEM AT THE PROPERTY LINE.

PUBLIC SPACE SITE PLAN NOTES:

- ALL EXISTING FEATURES ARE NOT NECESSARILY SHOWN ON THIS PLAN. SEE EXISTING CONDITIONS PLAN.
- THIS PLAN IS TO PERMIT WORK ON PUBLIC SPACE ONLY. ALL WORK SHOWN IN PRIVATE SPACE IS FOR INFORMATION PURPOSES ONLY. REFER TO THE SITE PLAN FOR WORK IN PRIVATE SPACE.
- REFER TO THE OTHER SHEET FOR ADDITIONAL INFORMATION.



1000 H ST NE
 SQ.- 958, LOT- 57

5-STORY WITH PARTIAL CELLAR AND PENTHOUSE MULTIFAMILY RESIDENTIAL BUILDING

DDOT UFA NOTES:

TREE PRESERVATION COMPONENTS FROM PERMIT TRACKING NUMBER 97041 MUST BE INSTALLED PRIOR TO ANY WORK OR STAGING ON THIS SITE. AND THAT A PRECONSTRUCTION MEETING MUST TAKE PLACE WITH URBAN FORESTRY AFTER TREE PROTECTION INSTALLATION.

- PRIVATE LEGEND**
- PROP. BUILDING - PRIVATE (126.19 X 94.97)
 - PROP. PERMEABLE PAVEMENT - PRIVATE
 - PROP. PERMEABLE PAVEMENT WITH PERMEABLE EASEMENT FOR 1016 H ST NE - PRIVATE
 - PROP. STANDARD PAVEMENT - PRIVATE UNDER BUILDING
- PROW LEGEND**
- PROP. STANDARD PAVERS (MATCH EX) PROW
 - PROP. GREEN SPACE (PROW)
 - PROP. TREE

CALL "MISS UTILITY" AT 1-800-257-7777
 48 Hours Before Start of Construction

H Street NE Public Art & Placemaking Initiative 2026

Draft PABC Proposal

Applicant: H Street Main Street

Contact: Anwar Saleem – Anwar.Saleem@HStreet.org – 202-439-0470

Funding Request

\$125,000

Project Period

October 1, 2026 – September 30, 2027

Project Overview

The H Street NE corridor is a designated Arts District with significant untapped potential. This project transforms public infrastructure into a cohesive, culturally rooted public art network through banners, murals, and wrapped utility boxes.

The initiative will enhance public safety and walkability, reduce visual blight and graffiti, celebrate H Street’s cultural identity, and strengthen its role as a destination Arts District.

Key Deliverables:

- 140 aluminum light pole banners
- 18 wrapped traffic/streetcar control boxes
- 6 large-scale murals
- Replacement of the “Phoenix” mural with a new flagship installation

Vision & Artistic Motivation

H Street Main Street envisions public art as essential infrastructure—not decoration, but a tool for cultural preservation, economic revitalization, community storytelling, and equitable placemaking.

This project transforms underutilized and neglected surfaces into high-impact visual landmarks reflecting Black cultural legacy, neighborhood resilience, and contemporary creativity.

Design Concept

The project uses a layered, corridor-wide design system:
Banners: Provide wayfinding and identity through thematic zones.
Traffic Box Wraps: Create street-level storytelling and engagement.
Murals: Serve as anchor cultural landmarks and destinations.

Community Impact

Serves approximately 250,000 residents and visitors annually.
Supports local artists, increases business visibility, enhances safety, and strengthens community identity across Ward 6 and beyond.

Timeline

Phase 1 (Oct–Dec 2026): Planning & Engagement
Phase 2 (Jan–Mar 2027): Fabrication
Phase 3 (Apr–Jul 2027): Installation
Phase 4 (Aug–Sept 2027): Activation

Sustainability

Banners: 8–10 years
Box wraps: 4–5 years
Murals: 10–15+ years

Includes annual maintenance and phased reinvestment strategy.

Budget Narrative

The total project budget is \$125,000 and includes all eligible expenses related to design, fabrication, installation, permitting, insurance, and community outreach.

Artist fees (\$24,500) support design and concept development.
Personnel (\$6,000) ensures project execution and oversight.
Insurance (\$1,500) covers general liability.

Fabrication (\$40,770) includes banners and vinyl wraps.
Installation (\$27,730) includes mural work and mounting.

Costs are based on verified vendor quotes and prior DC public art benchmarks.

All expenses reflect vendor estimates and prior project experience. A contingency is included within installation costs.



FLOWERS FOR MR. COLE
J.H. COLE DPR Park
MORSE & MONTELO St., NE
Washington, DC
06/16/19
Duijio Passariello
PROOF OF CONCEPT



LOVEJOY FLOWERS - LOVEJOY DPR Parke & 12 St., NE Washington, DC - 04/16/26 - Duijio Passariello

Government of the District of Columbia

Department of Transportation



April 10, 2026

Salvador Saucedo-Guzmán
Chair, Advisory Neighborhood Commission 5D05
Via email: 5D05@anc.dc.gov

NOI- 26-69-MSED

Re: Safety Treatment at Bladensburg Rd NE between Benning Rd and Neal St NE

Dear Chairperson:

Pursuant to the “Administrative Procedure Amendment Act of 2000” D.C. LAW 13-249 (48 DCMR 3491 April 20, 2001), the District Department of Transportation (DDOT) is required to give a written notice of our intent to modify traffic and/or parking requirements. This letter is being forwarded to you for the purpose of notification and solicitation of comments on our intent to implement the following modifications in your jurisdiction.

In response to 18 turning movement crashes, 4 bike/pedestrian crashes, and 2 head on collisions crashes from 2022-2024 and pursuant to DDOT’s selected preferred concept for the [Bladensburg Road NE Multimodal Safety and Access Study](#) completed in 2023, DDOT is proposing safety treatments on Bladensburg Rd NE at the intersections of Maryland Ave NE and Morse St NE. DDOT is proposing safety enhancements at the following locations:

- **New Parking restriction on Morse St NE**
 - 28 Ft Parking restriction (approximately 1 parking Space) in addition to 25 Ft minimum parking restriction required by DC Law at the intersection of Bladensburg Rd NE and Morse St NE to enhance intersection visibility for drivers making turns
- **New Left Turn restrictions on Bladensburg Rd NE**
 - Utilizing concrete median extensions to close median gaps on Bladensburg Rd from Maryland Ave to Neal St NE as outlined in the Bladensburg Road NE Multimodal Safety and Access Study
 - Allow Right turns only from private driveways into Bladensburg Rd in both directions from Maryland Ave to Neal St NE

The proposed treatments are intended to enhance safety for all roadway users by eliminating the main conflict points, or points at which vehicle paths cross, that contribute to the history of angle and head on crashes. By restricting left turns from private driveways onto Bladensburg Rd NE, DDOT expects to see reductions in minor crashes and injuries at these locations. Additionally, safety is enhanced for pedestrians by adding ADA compliant pedestrian crossings.



Figure 1: Proposed safety improvement at Bladensburg Rd NE and Maryland Ave NE

All comments on this subject matter must be filed in writing by **Tuesday, May 26, 2026**, (thirty business days after the date of this notice). To check the status of this Notice of Intent (NOI) or to submit a comment, please visit DDOT's NOI portal <https://noi.ddot.dc.gov/>. If you are having trouble accessing the NOI portal or would like to leave a comment via phone, please contact the DDOT Customer Service at 202-671-2800.

Sincerely,



Maris E. Fry, P.E., RSP1
Manager – Corridor Safety Projects Branch
Traffic Safety Administration, DDOT

CC:

Juan McCullum, Commissioner ANC 5D07
Nyasha Smith, Secretary to the Council of the District of Columbia
Christian Starghill, Manager, Ward 5 Liaison, Mayor’s Office of Community Relations and Services
Lacy Davis, Liaison, Ward 5 Liaison, Mayor’s Office of Community Relations and Services
Melissa Littlepage, Communications Director, Office of Councilmember Parker
Oliver Stoute, Director of Constituent Services, Office of Councilmember Parker
Andrianna Lovelace, Constituent Services Coordinator, Office of Councilmember Parker
Kisha M. Anderson, Ward 5 & 7 Community Engagement Specialist, DDOT
Tyler Williams, Community Engagement Manager, DDOT

Language Access Statement

The District Department of Transportation (DDOT) is committed to ensuring that no person is excluded from participation in, or denied the benefits of, its projects, programs, activities, and services on the basis of race, color, national origin, gender, age, or disability as provided by Title VI of the Civil Rights Act of 1964, the Americans with Disabilities Act and other related statutes. In accordance with the D.C. Human Rights Act of 1977, as amended, D.C. Official Code sec. 2-1401.01 et seq. (Act), the District of Columbia does not discriminate on the basis of actual or perceived: race, color, religion, national origin, sex, age, marital status, personal appearance, sexual orientation, gender identity or expression, familial status, family responsibilities, matriculation, political affiliation, genetic information, disability, source of income, status as a victim of an intrafamily offense, or place of residence or business. Sexual harassment is a form of sex discrimination which is prohibited by the Act. In addition, harassment based on any of the above protected categories is prohibited by the Act. Discrimination in a violation of the Act will not be tolerated. Violators will be subject to disciplinary action.

If you need language assistance services (translation or interpretation), please contact Taneshia Dublin,

Title VI, and Language Access Coordinator at (202) 997-7299 or ddot.languageaccess@dc.gov.

AYUDA EN SU IDIOMA

Si necesita ayuda en Español, por favor llame al 202-671-2700 para proporcionarle un intérprete de manera gratuita.

AVISO IMPORTANTE

Este documento contiene información importante. Si necesita ayuda en Español o si tiene alguna pregunta sobre este aviso, por favor llame al 202-671-2620. Infórmele al representante de atención al cliente el idioma que habla para que le proporcione un intérprete sin costo para usted. Gracias.

AIDE LINGUISTIQUE

Si vous avez besoin d'aide en Français appelez-le 202-671-2700 et l'assistance d'un interprète vous sera fournie gratuitement.

AVIS IMPORTANT

Ce document contient des informations importantes. Si vous avez besoin d'aide en Français ou si vous avez des questions au sujet du présent avis, veuillez appeler le 202-671-2700. Dites au représentant de service quelle langue vous parlez et l'assistance d'un interprète vous sera fournie gratuitement. Merci.

GIÚP ĐỠ VỀ NGÔN NGỮ

Nếu quý vị cần giúp đỡ về tiếng Việt, xin gọi 202-671-2700 để chúng tôi thu xếp có thông dịch viên đến giúp quý vị miễn phí.

THÔNG BÁO QUAN TRỌNG

Tài liệu này có nhiều thông tin quan trọng. Nếu quý vị cần giúp đỡ về tiếng Việt, hoặc có thắc mắc về thông báo này, xin gọi 202-671-2700. Nói với người trả lời điện thoại là quý vị muốn nói chuyện bằng tiếng Việt để chúng tôi thu xếp có thông dịch viên đến giúp quý vị mà không tốn đồng nào. Xin cảm ơn.

የቋንቋ ጸርዳታ

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ጠቃሚ ማስታወቂያ

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언어 지원

한국어로 언어 지원이 필요하신 경우 202-671-2700로 연락을 주시면 무료로 통역이 제공됩니다.

안내

이 안내문은 중요한 내용을 담고 있습니다. 한국어로 언어 지원이 필요하시거나 질문이 있으실 경우 202-671-2700 로 연락을 주십시오. 필요하신 경우, 고객 서비스 담당원에게 지원 받고자 하는 언어를 알려주시면 무료로 통역 서비스가 제공됩니다. 감사합니다.

語言協助

如果您需要用 (中文)接受幫助, 請電洽202-671-2700, 將免費向您提供口譯員服務

重要通知

本文件包含重要資訊。如果您需要用 (中文) 接受幫助或者對本通知有疑問, 請電洽202-671-2700。請告訴客戶服務部代表您所說的語言, 會免費向您提供口譯員服務。謝謝!



Government of the District of Columbia
Advisory Neighborhood Commission 5C
Arboretum | Brentwood | Fort Lincoln | Gateway | Langdon | Woodridge

**Resolution on Advancing Equitable Metrorail Access
through a New Ward 5 Rail Segment**

Sponsor: Commissioner Nelson (5C04)

Co-Sponsor: Commissioner Kapur (5C07)

Whereas:

- The Washington Metropolitan Area Transit Authority (WMATA) published the **Blue-Orange-Silver (BOS) Capacity and Reliability Study**¹ in 2019, with subsequent updates in 2021, evaluating long-term strategies to address Metrorail core capacity constraints, including conceptual build scenarios centered on a new Rosslyn river crossing and a new downtown core trunk
- Three of the four BOS build scenarios included new eastward Metrorail service extending through Ward 5, in addition to the proposed core capacity investments
- While these three alternatives varied in their specific alignment through DC and in their termini within Prince George's County, all included new stations notionally labeled Ivy City and Fort Lincoln, with continued service through Port Towns



Eastern expansion under WMATA BOS Alternatives 3, 5, and 6

¹ <https://www.wmata.com/initiatives/plans/BOS-Capacity-Reliability-Study/>

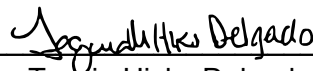
- The BOS build scenarios bundled multiple distinct infrastructure elements with a new Rosslyn crossing and downtown core trunk in order to maximize cross-jurisdictional participation in addressing a costly regional capacity challenge, without evaluating the costs and benefits of these individual elements in isolation
- In 2025, WMATA indicated a strategic shift toward advancing a lower-cost variation of the BOS framework focused on operational and near-term capacity improvements within the existing system, rather than proceeding with the previously studied new-rail build scenarios, orphaning Ward 5 segments despite repeated identification of this corridor as a viable expansion area in recognition of uniquely strong **equity, land-use, and ridership potential** among all expansion concepts
- WMATA's current capital program does not include any new Metrorail lines, extensions, or stations in design or construction, representing a departure from prior decades of system expansion and reinforcing the need to identify feasible, phaseable opportunities for future rail investment
- The Ward 5 segment identified in the BOS study, serving **Union Market, Ivy City, Langdon, Montana Triangle, Gateway, Arboretum, Woodridge, and Fort Lincoln**, leveraging existing railroad right-of-way to reduce construction costs and avoid displacement compared to deep-bore tunneling alternatives, could be studied and advanced as a distinct, phaseable investment independent of unresolved regional core capacity decisions, while remaining compatible with, and increasing stakeholders for, the future systemwide core capacity improvements that remain necessary, but not currently attainable
- Ongoing planning for the future of the RFK Stadium campus and surrounding East Capitol Street corridor presents a significant opportunity to coordinate land use and high-capacity transit investments across Ward 5 and adjacent areas, further underscoring the need for proactive Metrorail planning in Northeast DC
- Recent and anticipated redevelopment activity in the Hechinger Mall and Bladensburg Rd corridor, including the planned closure of the Safeway grocery store, highlights the need to coordinate redevelopment with improved high-capacity transit access to support neighborhood-serving retail, housing, and community stability
- Ward 5 continues to experience significant residential and employment growth in these communities while lacking comparable rapid transit access, and advancing a new Metrorail line in this corridor would promote equitable mobility, support adopted land-use plans, and expand the base of riders and stakeholders invested in the long-term performance of the Metrorail system
- The NoMa-Gallaudet Metrorail station demonstrates that targeted public-private partnership models, including coordinated participation by surrounding property owners and institutions, can successfully advance new Metrorail infrastructure where strong land-use and ridership potential exists through mixed-use, mixed-affordability revitalization of formerly industrial neighborhoods

- Active development decisions and Comprehensive Plan ambiguities, like those in ZC 24-11 which seeks to rezone nearly five acres of Montana Triangle from mixed-use residential/commercial to industrial-only use, threaten to eliminate thousands of potential homes, including affordable homes and lock key parcels along the proposed rail corridor into development patterns incompatible with the wellbeing of existing long-term residents' communities and future transit-oriented development, creating an urgent need for coordinated transit planning to inform land use decisions

Therefore, be it resolved that ANC 5C:

- **calls on the Washington Metropolitan Area Transit Authority (WMATA), in coordination with the District Department of Transportation (DDOT) and the DC Office of Planning (OP), to undertake focused study of a new Ward 5 Metrorail segment** consistent with corridors WMATA identifies in the BOS Capacity and Reliability Study
- **calls on the Mayor and Council to provide dedicated funding to such a study in the FY2027 capital budget**, with the understanding that the near-term project would specifically serve neighborhoods within DC
- **urges that such study evaluate a phaseable, District-focused rail investment serving Union Market, Ivy City, Langdon, Montana Triangle, Gateway, Arboretum, Woodridge, and Fort Lincoln**, independent of unresolved regional core capacity decisions, while remaining compatible with future systemwide improvements
- **encourages District agencies, WMATA, and stakeholders in areas surrounding the proposed Ward 5 rail segment to begin exploring potential funding and partnership models**, including lessons learned from the NoMa-Gallaudet infill station construction, should future study support advancement of the project
- **authorizes ANC 5C Commissioners Shawn Nelson (5C04) and VJ Kapur (5C07) to speak on behalf of the Commission** in advocating for the foregoing

Certification: Adopted on **4/9/2026** by a vote of **5-0**, with four commissioners required for quorum, at a regularly scheduled and duly noticed public meeting with a quorum present.



Tequia Hicks Delgado
Chair, ANC 5C